



MARION CENTRAL CORRIDOR DISTRICTS PLAN - 2019



ACKNOWLEDGEMENTS

We would like to specifically thank the following individuals for their significant commitment of time, effort, and energy to the project. The success of the project is a result of this commitment.

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Jillissa Moorman - Planning + Zoning Commission
Jill Ackerman - Marion Chamber of Commerce, President
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C. M. & St. P. Ry. Passenger Station, Marion, Iowa

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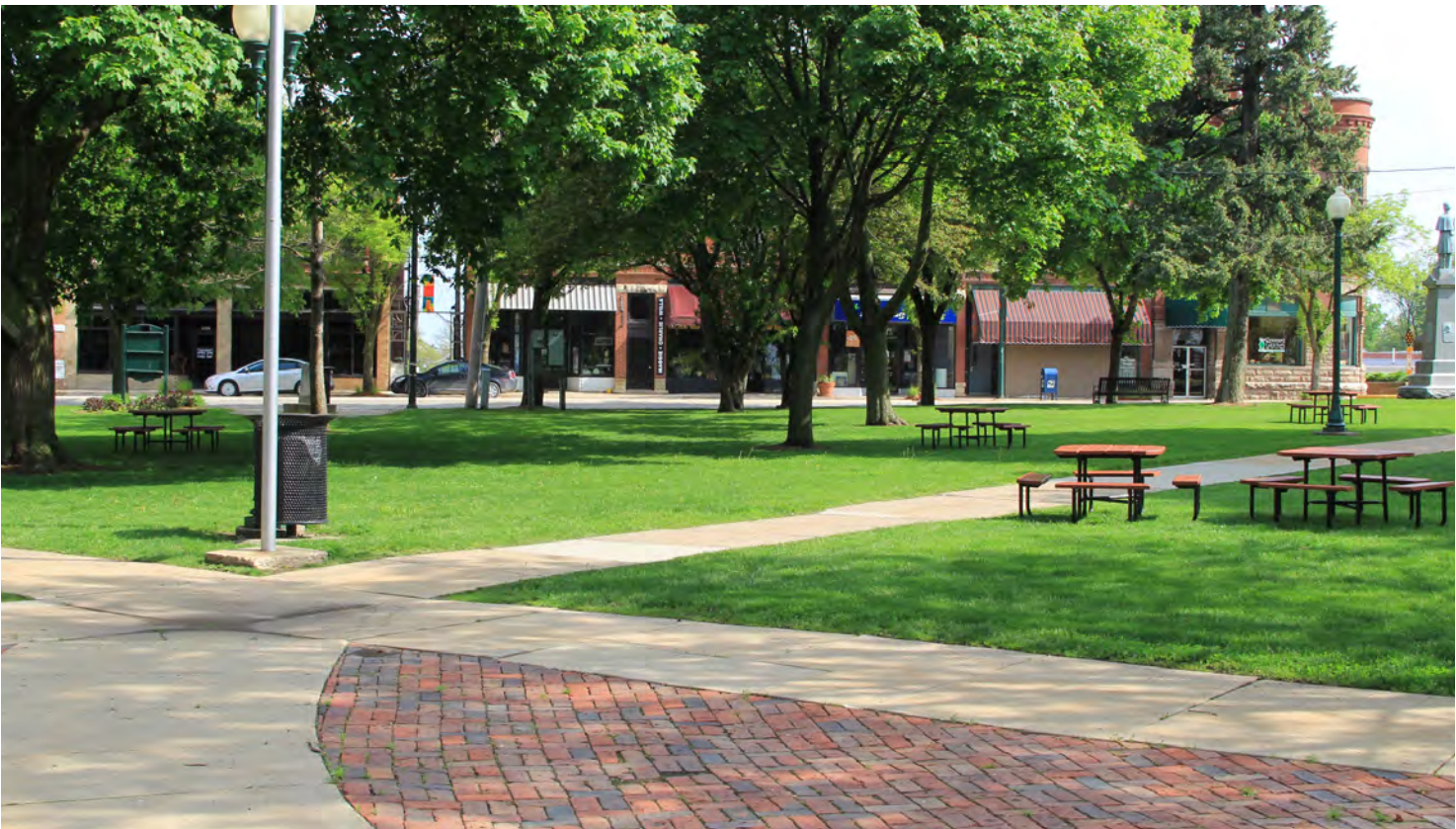
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PROJECT SUMMARY

General Overview:

Marion’s Central Corridor continues to be one of the most prominent and identifiable areas within the community and is rooted in the City’s history. Properties once used as warehouses and industrial uses associated with the railroad are now out of place as they reside adjacent to commercial and residential uses. Several properties are underutilized and ready for redevelopment. Over the past decade, Marion has been actively investing in the Corridor by creating the Marion Central Corridor Master Plan (adopted 2009), Uptown Streetscape Plan (adopted 2011) and the West End Master Plan (adopted 2011) which provide stepping stones to creating a unique regional business hub, urban-living housing options, and entertainment with a focus on walkability and multi-modal accommodations. Other investments include improvements to public infrastructure through the Central Corridor Improvement Project managed by the city’s Engineering Department.

Since the adoption of the Marion Central Corridor Master Plan in 2009, Marion has increased in population by approximately 4,300 residents from 2010 to 2017, built three roundabouts within the Central Corridor, and has reconstructed approximately half a mile of roadway along 6th Avenue. The Central Corridor encompasses the area between 2nd Street & 31st Street and between 5th Avenue and 9th Avenue, with 6th Avenue and 7th Avenue designated as the primary east/west thoroughfares. Through city initiatives, commercial redevelopment started occurring in 2013 and has increased in activity since.

To stay proactive and ensure the City and residents receive quality redevelopment, staff worked with Stanley Consultants, Inc. to update the Marion Central Corridor Master Plan with the Marion Central Corridor Districts

Plan (CCD). The updating process is an extension of the 2009 Plan and has built zoning districts and standards that will serve to implement the current vision as redevelopment occurs.

Existing Conditions:

The existing conditions of the corridor reflect the significant changes that have taken place over the years. In Marion’s Uptown, 7th Avenue reflects the historic architecture of the area, with a historic relationship of buildings to the street. In recent years, the 6th Avenue streetscape has been revitalized with new street paving, tree plantings and street furnishings. The remainder of the corridor does not have a discernible, formal relationship between the built structures and the street and pedestrian circulation. Even with redevelopment already occurring, there is still a mix of land uses left over from earlier eras that are no longer applicable in today’s economy. With the mix of existing land uses and the structure and parking configurations, the corridor has become primarily a vehicle-oriented corridor.

Revitalizing the Central Corridor offers challenges with the variety of land uses and the orientation/relationship of structures to the street and pedestrian circulation. However, the corridor also offers many welcoming features with the Central Business Historic District, the Uptown Artway, many locally owned businesses, and a renewed interest of developers to build within the Central Corridor.

Project Challenges:

As the City of Marion has evolved, it has become apparent that it is time to update the zoning for the Central Corridor to reflect current and future desired land uses, aesthetic values, and circulation for both vehicular and pedestrian modes of transportation. Significant project challenges include:

- Lack of urban form and density to stimulate the economy of the corridor
- Lack of a critical mass of residential units in the corridor
- The need for diversity of land uses that support a vibrant urban economy
- Pedestrian-safety concerns
- Warehouse era land uses
- Vehicular dependence
- Incompatible land uses and lot sizes
- Overhead utilities
- Undesirable deteriorating aesthetics
- Little continuity in design standards, building orientation, and land uses
- Building on the vision of the corridor started with the 6th Avenue streetscape improvements and needs momentum to continue.



Planning Process:

A Central Corridor Advisory Committee was established to provide community input and ensure that the community vision was incorporated into the districts and proposed standards. Through the planning process, zoning districts were created within the Central Corridor to support the 2009 Central Corridor Master Plan. The zoning districts are modeled after the modern zoning concept known as Form-Based Code. Form-Based Code is a land development regulation (zoning) that fosters predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code. The vision for the Central Corridor, with emphasis on 7th Avenue and 6th Avenue between the west end and east end roundabouts, is to foster a pedestrian environment and street-oriented design.

Central Corridor Assessment Map (Page 7)

One of the first exercises in the planning process was to understand the Central Corridor environment. The Central Corridor Assessment map is a compilation of information collected about the Central Corridor. The plan documents current landmarks, circulation patterns, current roundabouts, Pucker Street Historic District, and potential gateway opportunities. This plan was used to understand and map the character of the corridor. It was used in discussions with the Steering Committee and Planning Staff to identify the new zoning districts, and ultimately the Central Corridor boundaries.

Central Corridor Current Zoning Map (Page 8)

The current zoning within the Central Corridor consists primarily of commercial and warehouse zoned parcels. This is a reflection of the rail industry history. At its core, the Central Business District zoning, C-2, represents the historic core of the Central Corridor.

Central Corridor District Map (Page 9)

The districts being proposed will preserve the Central Business Historic District, promote density and residential around the central business district, provide transitional uses adjacent to residential neighborhoods and specifically define the permitted uses desired in the Central Corridor.

Central Corridor Illustrative (Page 10)

The Central Corridor Illustrative is a conceptual representation of the corridor if it were generally developed in accordance with the proposed zoning regulations. Since it is impossible to predict how the corridor will develop, the purpose of the illustrative is to graphically portray the intent of the zoning regulations. Once the regulations are in place and the planning staff begin to work with developments, amendments and further design standards may be required to guide the aesthetic of development.

Zoning Districts:

Seven (7) zoning districts were designed to regulate land uses and urban form within each district of the Central Corridor. The districts, identified in the CCD Plan, subdivide the corridor into areas of similar characteristics and urban form. These characteristics and urban forms are defined by historic context, building orientation, density, building height, and permitted land uses. The zoning districts include: Uptown 1 (U-1), Uptown 2 (U-2), Urban Transition Residential 1 (UTR-1), Urban Transition Residential 2 (UTR-2), Urban Transition Commercial 1 (UTC-1), Urban Transition Commercial 2 (UTC-2), and Urban Commercial 1 (UC-1).

The U-1 district encompasses the area historically known as the Central Business District (CBD) and includes the Central Business Historic District. Moving outward from the U-1 district, the development standards and land uses for each district change to foster different built results that are compatible with their surroundings. Generally, the U-2 District is the most intensive urban environment, while the UC-1 District is the least intensive built environment. All the districts encourage urban forms that support a pedestrian environment and street-oriented design.



In every district, regulations either require or encourage the building to be placed close to the street. The desired effect is a return to buildings that are directly accessible to pedestrians as seen in the historic areas of the CBD. Regulations will direct parking areas to be located at the rear of the lot, behind, or to the side of buildings. As redevelopment occurs, 7th Avenue is redesigned, and 6th Avenue constructed, the corridor will become pedestrian friendly. For the area between the 7th Avenue / 7th Street roundabout and the 6th Avenue / 15th Street roundabout, the built environment will not only encourage pedestrian activity, but increase residents living, working and shopping along the corridor.

Review Process:

Any proposed substantial improvements to a building or new building structure within the Central Corridor shall adhere to the standards of the zoning district in which it is located. The applicant shall submit a detailed site plan, colored renderings, elevations, material list and building plan for review and approval from the Planning and Zoning Commission and City Council prior to improvements being made. Two additional review

areas are included within the Central Corridor, the Uptown Marion Sub-District Review and the Historic Sub-District Review. Properties located within these sub-districts shall be reviewed by the Uptown Marion Street Board of Directors, the Historic Preservation Commission, or their respective designee which will provide recommendation to the Planning and Zoning Commission and City Council.

Substantial improvements to an existing structure include any renovation that involves modifications of the exterior appearance of the structure by virtue of adding or removing exterior windows or doors or altering the color or exterior materials of existing wall. An increase or decrease in the existing building height and/or alteration of the existing roof pitch or appearance also constitutes a substantial improvement. Awning structures or similar material extensions over the public sidewalk area that are new, modified, or replacements shall go through the review process.

Routine repair or replacement of existing roof materials that do not materially change the appearance, shape, or configuration of the existing roof will not be considered a substantial improvement and will not be subject to these regulations.

Owner-occupied detached single-family residences shall not be subject to these regulations.

Implementation:

The Marion Central Corridor Districts Plan 2019 shall be adopted by the City Council with the zoning districts and design standards approved by Ordinance and placed within the Marion Code of Ordinances Chapter 176, Zoning Regulations as Section 176.50, Central Corridor Zoning Districts.

The regulations and guidelines established by these zoning districts shall apply to the Central Corridor Districts as shown on the Official Zoning Map of Marion.

CENTRAL CORRIDOR ASSESSMENT MAP - 2018 SNAPSHOT

CITY OWNED

SCHOOL OWNED

PUCKER STREET HISTORIC RESIDENCE

CHURCH OWNED

FUTURE PLAZA

ROUNDBABOUT

GATEWAY OPPORTUNITY

DISTRICT LANDMARKS

BOUNDARY STUDY AREA

LIMITED ACCESS

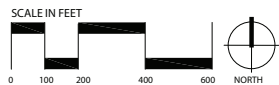
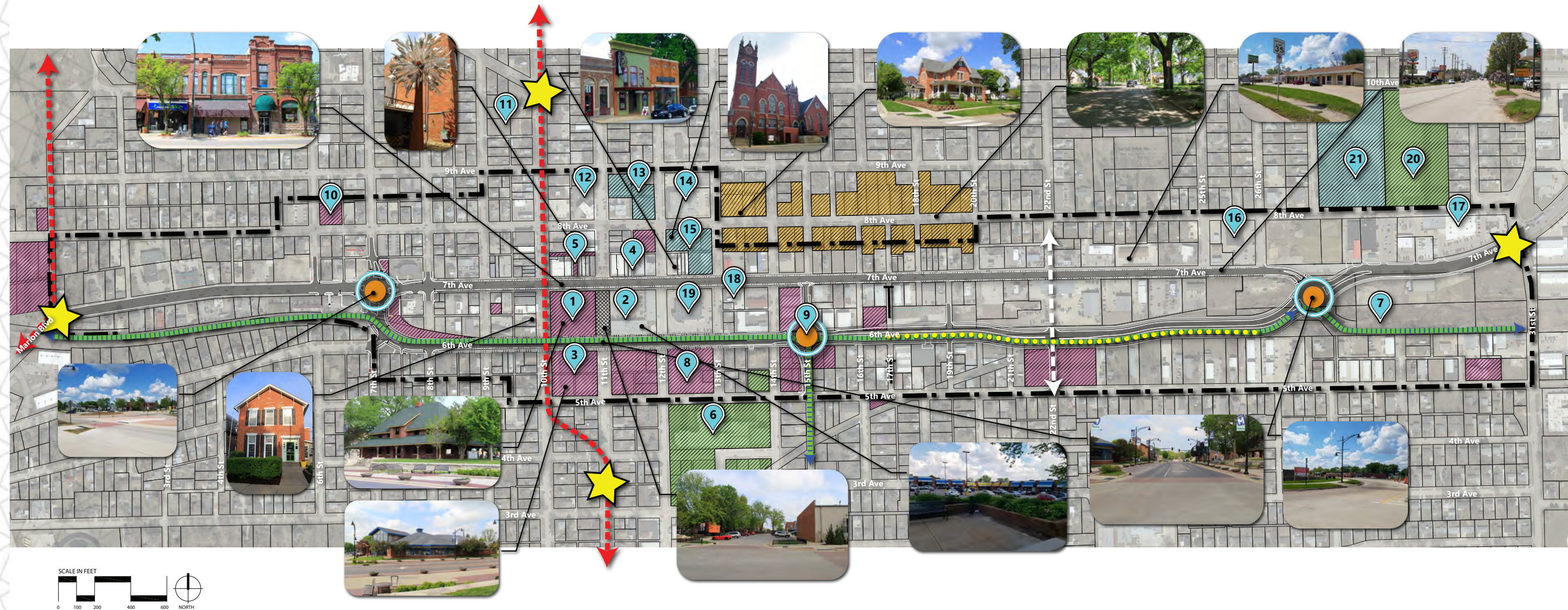
POTENTIAL ARTERIAL IMPROVEMENTS

FUTURE BIKE ROUTE

FUTURE DEAD ENDS

FUTURE CONNECTIONS

DISTRICT LANDMARKS			
1	City Square Park	12	Hills Bank and Trust
2	Mixed Use Project	13	1st Presbyterian Church
3	Existing Library - Redevelopment Site	14	Farmers State bank
4	Historic District	15	1st United Methodist Church
5	Uptown Artway	16	Dollar Tree
6	Vernon Middle School	17	Unity Point Clinic
7	Hotel Complex	18	Phillips Center
8	City Hall	19	Walgreens
9	Roundabout	20	Longfellow Elementary
10	Fire Station #1	21	St. Pauls Lutheran Church
11	Granger House Museum		



Note: The Assessment area encompasses a larger study area than the ultimate final district area.

CENTRAL CORRIDOR CURRENT ZONING MAP

LEGEND

- R-5 (MODERATE DENSITY MULTIPLE-FAMILY RESIDENTIAL)
- PD-S (PLANNED DEVELOPMENT SPECIAL)
- O-1 (OFFICE/TRANSITIONAL)
- C-1 (NEIGHBORHOOD COMMERCIAL)
- C-2 (CENTRAL BUSINESS DISTRICT COMMERCIAL)
- C-3 (GENERAL COMMERCIAL)
- C-4 (WAREHOUSE/COMMERCIAL)
- I-1 (RESTRICTED INDUSTRIAL)
- I-2 (GENERAL INDUSTRIAL)

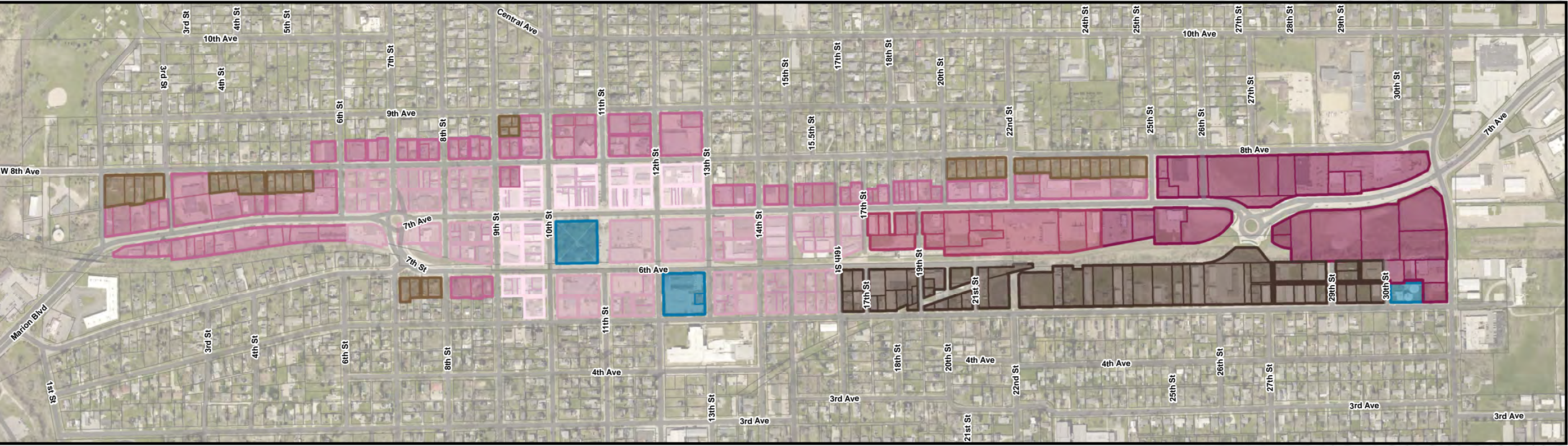


CENTRAL CORRIDOR DISTRICT MAP

LEGEND

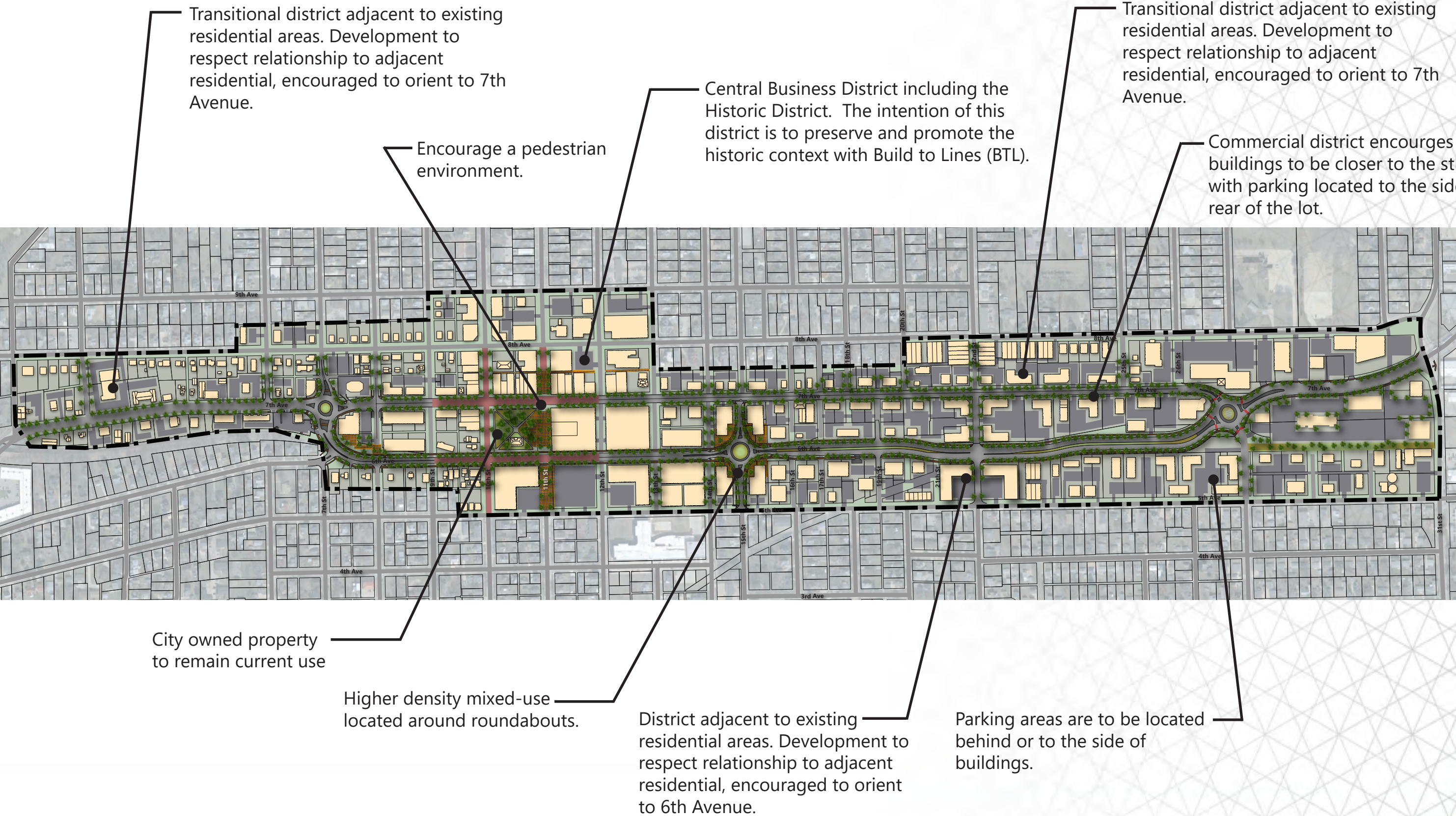
- U-1 (Uptown 1)
- UTR-1 (Urban Transition Residential 1)
- UTC-1 (Urban Transition Commercial 1)
- UC-1 (Urban Commercial)
- U-2 (Uptown 2)
- UTR-2 (Urban Transition Residential 2)
- UTC-2 (Urban Transition Commercial 2)
- PI* (Public Institutional)

N
1 inch = 600 feet



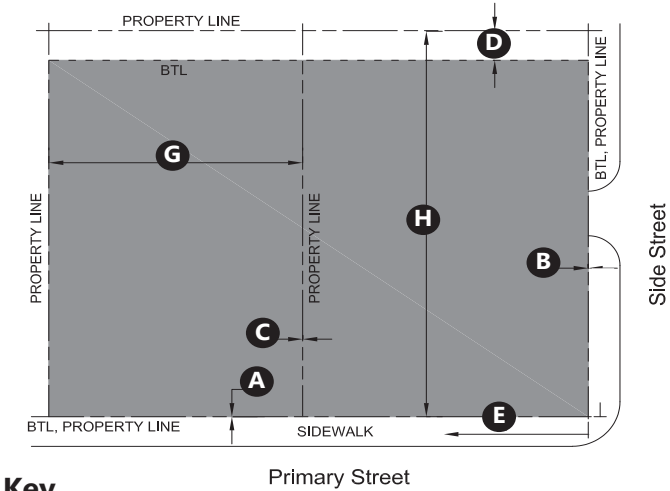
* Public Institutional zoning district is not included within the Central Corridor Districts Plan.

CENTRAL CORRIDOR ILLUSTRATIVE



UPTOWN 1 (U-1) DISTRICT STANDARDS

The U-1 Uptown District is the Central Business District which includes the Central Business Historic District. The intent of the U-1 district is to preserve and promote the quality of life and historic business district. These regulations are intended to preserve the historic buildings and in turn ensure the continued commercial viability by maintaining the use of quality building materials, encouraging pedestrian traffic, and preserving and enhancing community gathering spaces. The historic reference for this type of urban form is seen on the north side of 7th Avenue, between 10th and 13th Street. As such, new development and preservation of existing buildings are required to emulate this historic urban form.



Key

-----	PROPERTY LINE
-----	BUILD-TO LINE (BTL)

Building Placement

Build-to-Line (Distance from Property Line)		
Front	0'	A
Side Street, Corner Lot	0'	B

Setback

Side	0'	C
Rear		
Adjacent to Residence	15' min.	D
Adjacent to any other use	10' min.	D

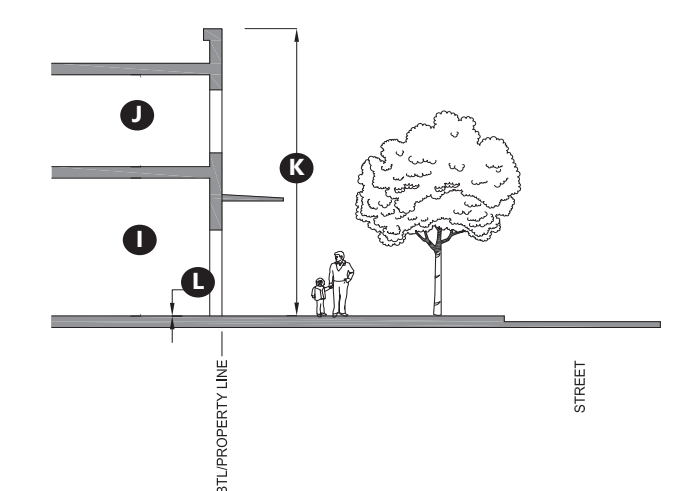
Building Form

Primary Street - Bldg. frontage on BTL	90% min.*	E
Lot Width	18' min.	G
Lot Depth	120' min.	H
Lot Coverage	90% max.	

* Street facades must be built to BTL within 30' of every corner.

Uses Per Floor

1st Floor	Retail/Service, Cultural, Recreational & Entertainment, Government & Institutional	I
Upper Floors	Residential, Personal Service	J

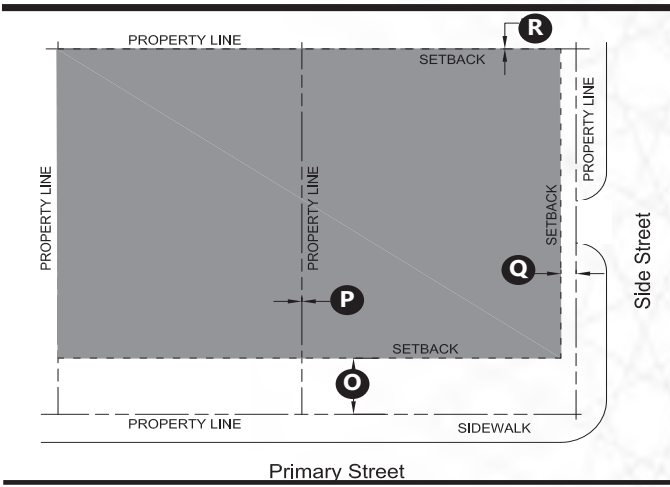


Height

Building Minimum	2 stories	K
Building Maximum	4 stories	K
Ground Floor Finish Elevation	12" max. above sidewalk	L

Notes

- All floors must have a primary ground-floor entrance that faces primary or side street.
- Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.
- Any building over 60' wide must be broken down to read as a series of buildings no wider than 60' each.
- Mansard roof forms are not allowed.
- Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.
- Any section along the BTL not defined by a building must be defined by a 4'6" high architectural element.



Key

-----	PROPERTY LINE
-----	BUILD-TO LINE (BTL)

Parking

Location (Distance from Property Line)

Front Setback	20'	O
Side Setback	0'	P
Side Street Setback*	5'	Q
Rear Setback	0'	R

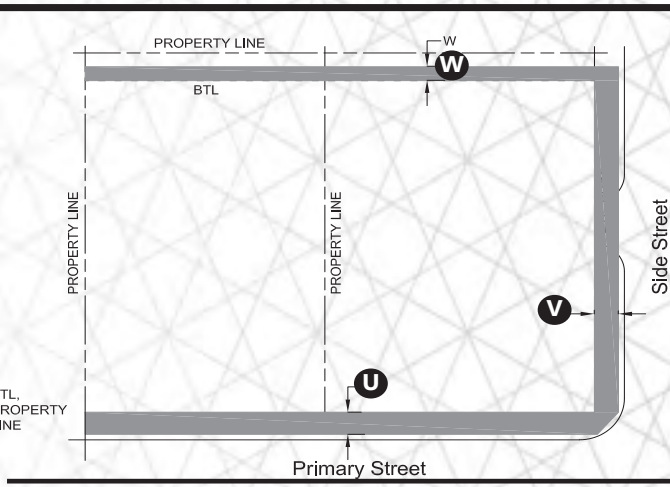
* Setback may be waived when a screening element is provided in the form of a solid wall, or densely planted compact hedge. Such screening shall be not less than four (4) feet in height and shall be adequately maintained. The design of the screening element is subject to approval by the Community Development Director.

Required Parking Spaces

Off-street parking is not required.

Notes

- Parking not allowed between building and curb.
- On corner lots, primary parking drive shall not be located on primary street.
- Connectivity is encouraged between adjacent lots.
- Parking may be provided off-street within 300' or as shared parking.



Key

-----	PROPERTY LINE
-----	BUILD-TO LINE (BTL)

Setback Encroachments

Location

Front	8' max.	U
Side Street	8' max.	V
Rear	5' max.	W

Notes

Canopies, awnings, and balconies may encroach over the BTL into the right-of-way on the street sides and into the setback on the rear, as shown in the shaded areas. Encroachments shall not be within two (2') feet of the back-of-curb (BOC).

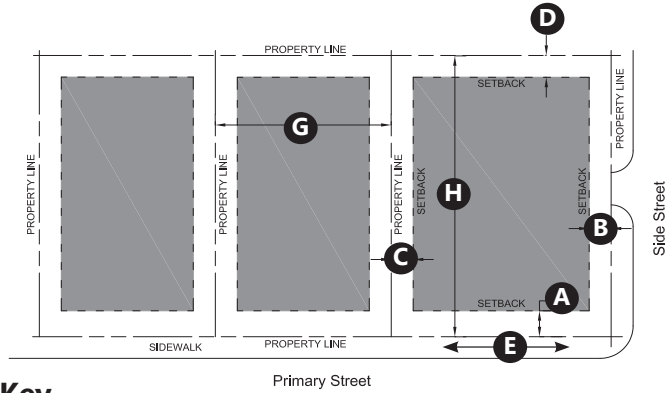
Special exceptions may be allowed with the approval of the Community Development Director.

Frontage Types

Forecourts		
Depth	20' min., not to exceed width	
Width	20' min., 50% of lot width max.	
Stoops		
Depth	4' min., 6' max.	
Storefront		
Depth	0' min., 6' max.	

UPTOWN 2 (U-2) DISTRICT STANDARDS

The U-2 Uptown District is located adjacent to the U-1 District and in context with the roundabouts at 7th Street and 15th Street. The U-2 District is intended to be a more intensive use district than the U-1 District and allows for full block buildout and higher densities. The District encourages residential, service, and retail uses to develop in a mixed-use environment that supports Uptown living, shopping and working. Similar to the U-1 District, quality materials, and pedestrian circulation and amenities are encouraged to provide community gathering spaces.



Key

PROPERTY LINE

SETBACK LINE

BUILDING ENVELOPE

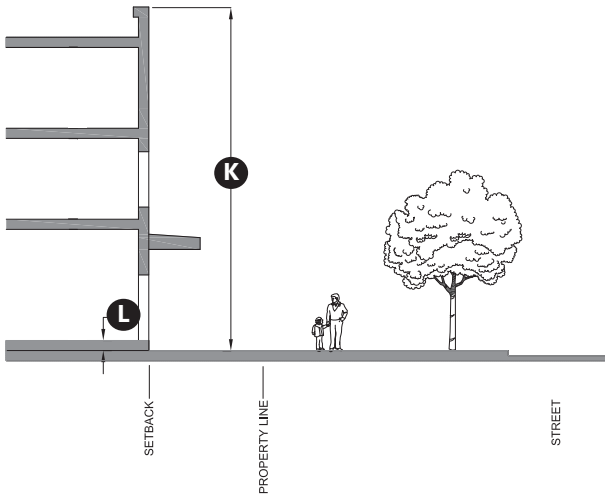
Building Placement

Setback (Distance from Property Line)			
Front	12' max.	A	
Side Street, Corner Lot	12' max.	B	
Side	12' max.	C	
Rear			
Adjacent to Residence	15' min.	D	
Adjacent to any other use	10' min.	D	

Building Form

Primary Street - Percent of bldg. on SB line	80%	E
Lot Width	18' min.	G
Lot Depth	120' min.	H
Lot Coverage	90% max.	

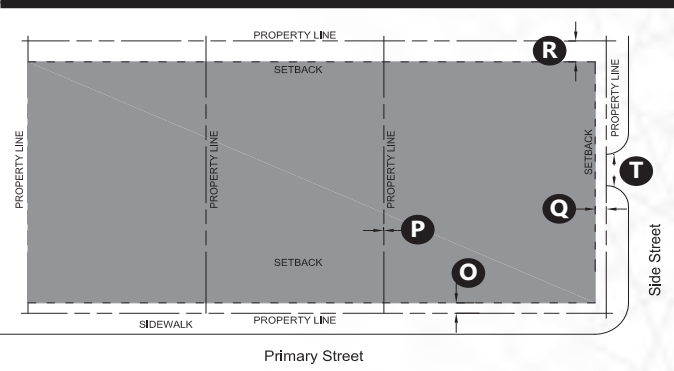
Notes	
N/A	



Height

Building Minimum	2 stories	K
Building Maximum	4 stories	K
Ground Floor Finish Elevation	12" max. above sidewalk	L

Notes	
All floors must have a primary ground-floor entrance that faces primary or side street.	
Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.	
Any building over 75' wide must be broken down to read as a series of buildings no wider than 75' each.	
Mansard roof forms are not allowed.	
Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.	



Parking lot screening per Sec. 176.29-2.H.(7)

Key

PROPERTY LINE

PARKING AREA

Parking

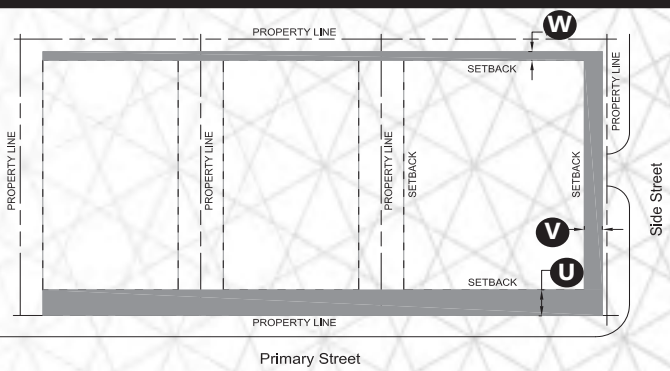
Location (Distance from Property Line)		
Front Setback*	5'	O
Side Setback	0'	P
Side Street Setback*	5'	Q
Rear Setback	10'	R

* Setback may be waived when a screening element is provided in the form of a solid wall, or densely planted compact hedge. Such screening shall be not less than four (4) feet in height and shall be adequately maintained. The design of the screening element is subject to approval by the Community Development Director.

Required Parking Spaces

Off-street parking is not required.

Notes	
Parking not allowed between building and curb.	T
On corner lots, primary parking drive shall not be located on primary street.	
Connectivity is encouraged between adjacent lots.	
Parking may be provided off-street within 300' in parking lots.	



Key

PROPERTY LINE

ENCROACHMENT AREA

SETBACK LINE

Setback Encroachments

Location		
Front	8' max. into the setback area.	U
Side Street	8' max. into the setback area.	V
Rear	5' max. into the setback area.	W

Notes	
Storefronts, Stoops, balconies, bay windows and awnings may encroach into the setback area.	

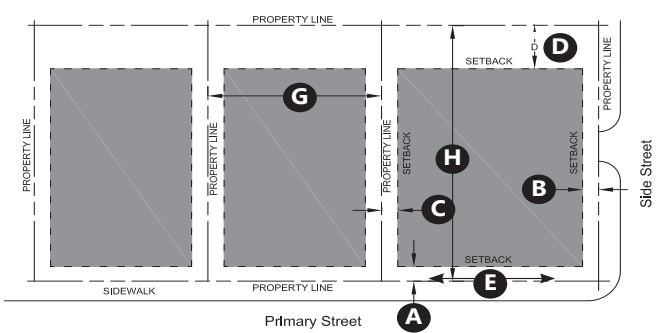
Frontage Types

Terrace	Depth	8' min.
Forecourts	Depth	20' min., not to exceed width
	Width	20' min., 50% of lot width max.
Stoops	Depth	4' min., 6' max.
Storefront	Depth	0' min., 6' max.

Notes	
Landscape buffer is required between U-2 and adjacent residential, consistent with Section 176.43	

URBAN TRANSITION RESIDENTIAL 1 (UTR-1) DISTRICT STANDARDS

The UTR-1, Urban Transition Residential District is a transitional zoning district located between 7th and 8th Avenues. This District is adjacent to exiting low density residential areas north of 8th Avenue and generally shares a rear property line with more intense commercial development adjacent to 7th Avenue. The intent of the district is to promote residential development in medium densities to serve as a buffer between commercial development and primarily low density residential neighborhoods. Lower density multi-unit development is encouraged in the district.



Key		
-----	PROPERTY LINE	
-----	SETBACK LINE	
	BUILDING ENVELOPE	

Building Placement

Setback (Distance from Property Line)		
Front	7' min.; 15' max.*	A
Side Street, Corner Lot	7' min.; 15' max.	B
Side	7' min.	C
Rear	20' min.	D

* 20' to front of garage.

Building Form

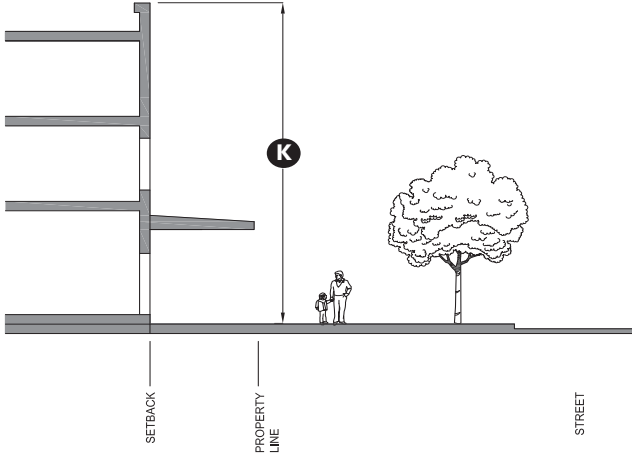
Primary Street - Percent of bldg. on SB line	60%	E
Lot Width	18' min.; 120' max.	G
Lot Depth	120' max.	H
Lot Coverage	70 %	

Accessory Buildings

Front Setback	20' plus front yard
Side Setback	5' min. (or existing building setback)
Rear Setback	5' min.; 10' to an alley

Notes

N/A



Height

Building Minimum	1 story	K
Building Maximum	3 stories	K
Accessory Building Max.	1 story	

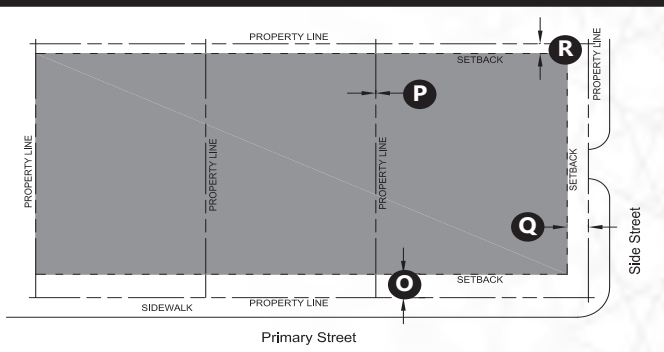
Notes

Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.

Upper floor residential units must have a visible ground floor entrance that faces the street.

Mansard roof forms are not allowed.

Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.



Key		
-----	PROPERTY LINE	
-----	SETBACK LINE	
	PARKING AREA	

Parking

Location (Distance from Property Line)		
Front Setback	12'	O
Side Setback	0'	P
Side Street Setback	10'	Q
Rear Setback	5'	R
Adjacent Residential	15' w/buffer	R

Required Parking Spaces*

1 to 2 family residential	1.0 / dwelling
3 or more family residential	1.5 / dwelling
3 or more bedrooms	2.0 / dwelling
Retail/Service	3.5 space/1,000 GSF
Lodging	1.0 space/room

* Reference Section 176.29-2 Parking and Loading for uses not specified; Parking lot screening per Secection 176.29-2.H.(7); For Mixed Use developments, parking requirements can be reduced by the shared parking factor.

Notes

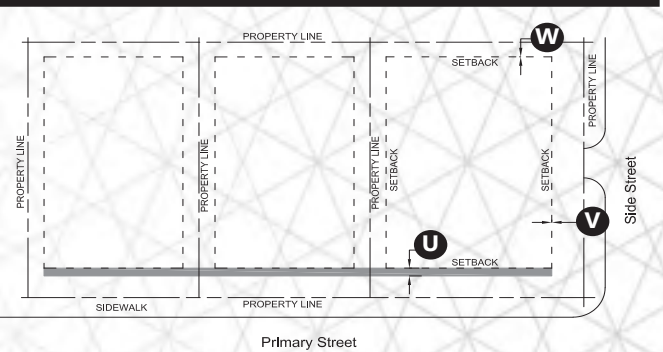
On corner lots, primary parking drive shall not be located on primary street.

Parking not allowed between primary building and curb. Single Family front loaded garages excluded.

Connectivity is encouraged between adjacent lots.

Parking only allowed to the back or side of building or in parking garage which is to be located to the back or side of building.

Landscape buffer is required between UTR-1 and Uptown/Commercial Districts and adjacent residential.



Key		
-----	PROPERTY LINE	
-----	SETBACK LINE	
	ENCROACHMENT AREA	

Setback Encroachments

Location		
Front	8' max.	U
Side Street	0'	V
Rear	0'	W

Notes

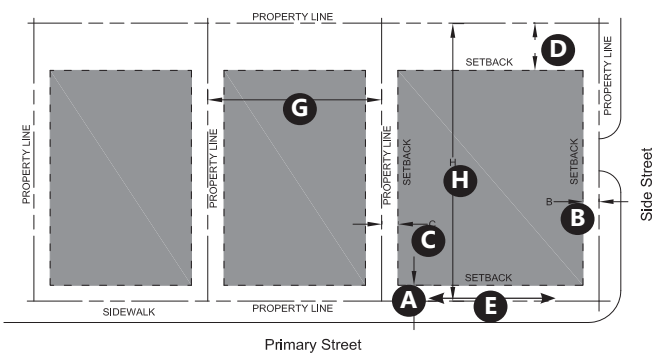
Porches, balconies, bay windows and awnings, may encroach into the setback.

Frontage Types

Common Lawn	Depth	Within the front setback
Porches	Depth	8' min.
	Height	1 story max.
Terrace	Depth	8' min.
Forecourts	Depth	20' min., not to exceed width
	Width	20' min., 50% of lot width max.
Stoops	Depth	4' min., 6' max.

URBAN TRANSITION RESIDENTIAL 2 (UTR-2) DISTRICT STANDARDS

The UTR-2, Urban Transition Residential District is located between 5th and 6th Avenue; east of 15th Street. Formerly containing warehouse and light industrial uses and is directly adjacent to existing medium density residential areas. The intent of the district is to encourage medium and high density residential development with limited support commercial activities that would buffer the existing single-family neighborhoods from more intensive commercial development along 6th Avenue. Development is encouraged to respect the relationship to the existing lower density residential neighborhoods south of 5th Avenue. Medium and high-density developments are encouraged to orient to 6th Avenue.



Key		
-----	PROPERTY LINE	BUILDING ENVELOPE
- - -	SETBACK LINE	

Building Placement

Setback (Distance from Property Line)		
Front	7' min.; 15' max.*	A
Side Street, Corner Lot	7' min.; 15' max.	B
Side	7' min.	C
Rear	20' min.	D

*20' to front of a garage.

Building Form

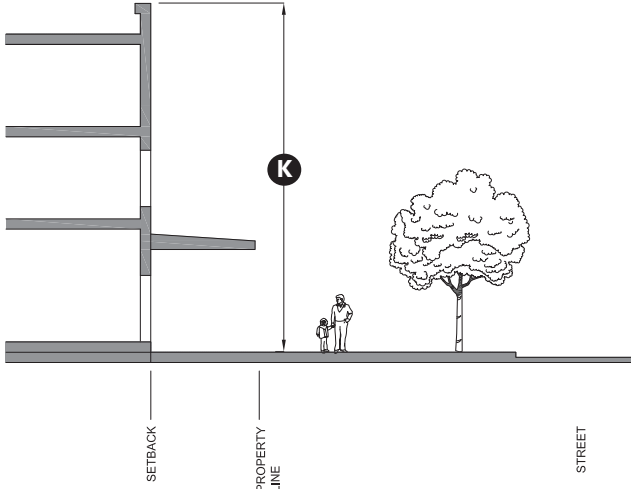
Primary Street - Percent of bldg. on SB line	60%	E
Lot Width	18' min.	G
Lot Depth	NA	H
Lot Coverage	70%	

Accessory Buildings

Front Setback	20' plus front yard
Side Setback	5' min. (or existing building setback)
Rear Setback	5' min.; 10' to an alley

Notes

N/A



Height

Building Minimum	1 story	K
Building Maximum	4 stories	K
Accessory Building Max.	1 story	

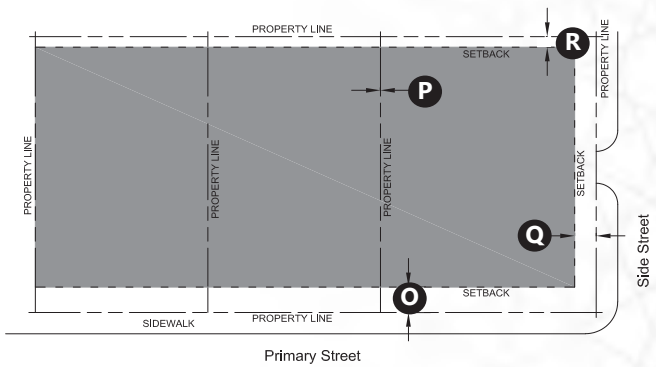
Notes

Rear-facing buildings, and service entries are prohibited on street-facing facades.

Upper floor residential units must have a visible ground floor entrance that faces the street.

Mansard roof forms are not allowed.

Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.



Key		
-----	PROPERTY LINE	
-----	PARKING AREA	

Parking

Location (Distance from Property Line)

Front Setback	12'; same as building	O
Side Setback	0'	P
Side Street Setback	10'	Q
Rear Setback	5'	R
Adjacent Residential	15' w/buffer	R

Required Parking Spaces*

1 to 2 family residential	1.0 / dwelling
3 or more family residential	1.5 / dwelling
3 or more residential bedrooms	2.0 / dwelling
Commercial/Retail/Service	3.0 space/1,000 GSF

* Reference Section 176.29-2 Parking and Loading for uses not specified; Parking lot screening per Section 176.29-2.H.(7); For Mixed Use developments, parking requirements can be reduced by the shared parking factor.

Notes

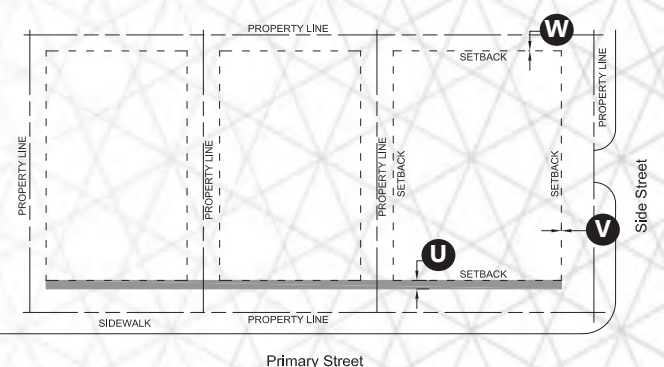
On corner lots, primary parking drive shall not be located on primary street.

Parking not allowed between primary building and curb. Single Family front loaded garages excluded.

Connectivity is encouraged between adjacent lots.

Parking only allowed to the back or side of building or in parking garage which is to be located to the back or side of building.

Landscape buffer is required between UTR-2 and Uptown/Commercial Districts and adjacent residential.



Key		
-----	PROPERTY LINE	ENCROACHMENT AREA
- - -	SETBACK LINE	

Setback Encroachments

Location

Front	8' max.	U
Side Street	0'	V
Rear	0'	W

Notes

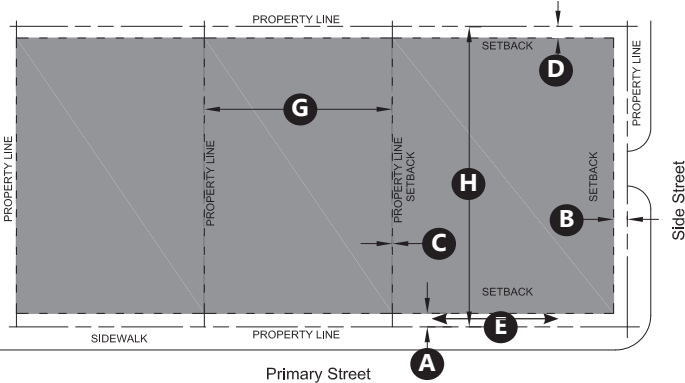
Porches, balconies, bay windows, stoops and awnings, may encroach into the setback.

Frontage Types

Common Lawn	Depth	Within the front setback
Porches	Depth	8' min.
	Height	1 story max.
Terrace	Depth	8' min.
Forecourts	Depth	20' min., not to exceed width
	Width	20' min., 50% of lot width max.
Stoops	Depth	4' min., 6' max.

URBAN TRANSITION COMMERCIAL 1 (UTC-1) DISTRICT STANDARDS

The UTC-1, Urban Transition Commercial District is a transitional zoning district located on the north side of 7th Avenue south of Pucker Street Historic District and on either side of 7th Avenue west of 6th Street. The district provides a buffer to residential areas north of the traditional Central Business District. The intent of the district is to promote commercial development which respects the relationship to adjacent residential neighborhoods. Development will be encouraged to orient entrances to the primary street frontage and respect and be compatible with adjacent residential uses.



Key

PROPERTY LINE

SETBACK LINE

BUILDING ENVELOPE

Building Placement

Setback (Distance from Property Line)		
Front	5' min.; 15' max.	A
Side Street, Corner Lot	5' min.; 15' max.	B
Side	0'	C
Rear		
Adjacent to Residence	15'	D
Adjacent to any other use	5'	D

Building Form

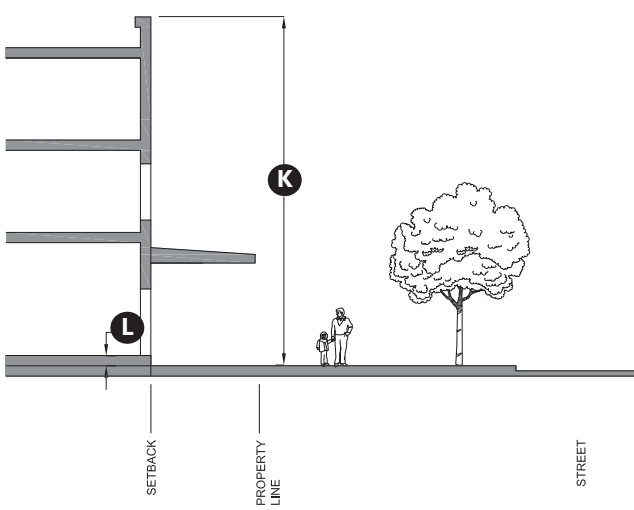
Primary Street - Bldg. Frontage on Setback	60%	E
Lot Width	300' max.	G
Lot Depth	120' max.	H
Lot Coverage	70% max.	

Accessory Buildings

Front Setback	20' plus front yard
Side Setback	5' min. (or existing building setback)
Rear Setback	5' min.; 10' to an alley

Notes

N/A

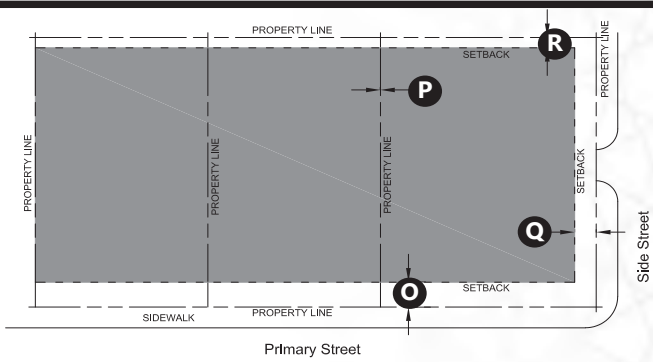


Height

Building Maximum	3 stories	K
Accessory Building Max.	1 story	
Ground Floor Finish Elevation	12" max. above sidewalk	L

Notes

- Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.
- Any building over 75' wide must be broken down to read as a series of buildings no wider than 75' each.
- Upper floor residential units must have a visible ground floor entrance that faces the street.
- Mansard roof forms are not allowed.
- Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.



Key

PROPERTY LINE

SETBACK LINE

PARKING AREA

ENCROACHMENT AREA

Parking

Location (Distance from Property Line)		
Front Setback	Same as building	O
Side Setback	0'	P
Side Street Setback	10'	Q
Rear Setback	5'	R
Adjacent Residential	15' w/buffer	

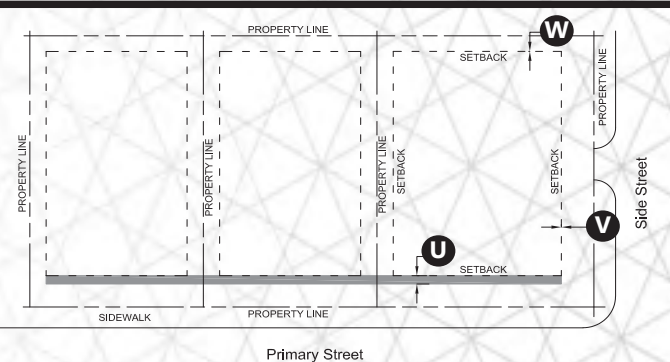
Required Parking Spaces*

Multi-Unit, Upper Level Dwellings	1.5 space/unit
Commercial/Retail/Service	3.5 space/1,000 GSF
Medical Office	1.0 space/150 GSF
Lodging	1.0 space/room
Bicycle Parking	1.0 space/10 vehicle space

* Reference Section 176.29-2 Parking and Loading for uses not specified; Parking lot screening per Section 176.29-2.H.(7); For Mixed Use developments, parking requirements can be reduced by the shared parking factor.

Notes

- On corner lots, primary parking drive shall not be located on primary street.
- Parking not allowed between Primary building and curb.
- Connectivity is encouraged between adjacent lots.
- Parking only allowed to the back or side of building or in parking garage which is to be located to the back or side of building.
- Landscape buffer is required between UTC-1 and adjacent residential.



Key

PROPERTY LINE

SETBACK LINE

ENCROACHMENT AREA

Setback Encroachments

Location		
Front	4' max.	U
Side Street	0' max.	V
Rear	0' max	W

Notes

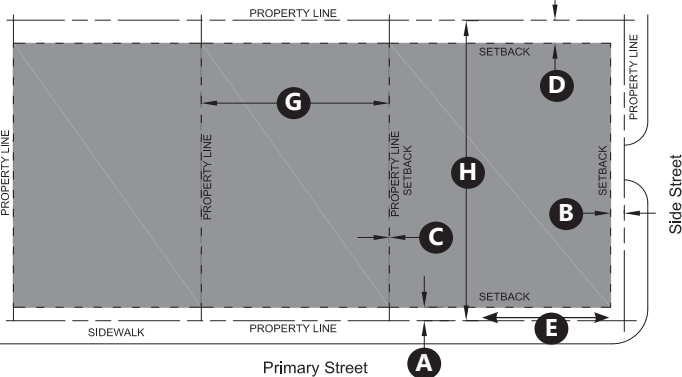
Porches, commercial storefronts, balconies, bay windows, stoops and awnings, may encroach into the setback, as shown in the shaded areas.

Frontage Types

Awnings		
Depth	10' max.	
Forecourts		
Depth	20' min., not to exceed width	
Width	20' min., 50% of lot width max.	
Stoops		
Depth	4' min., 6' max.	
Porches		
Depth	8' min.	
Height	1 story max.	

URBAN TRANSITION COMMERCIAL 2 (UTC-2) DISTRICT STANDARDS

The UTC-2, Urban Transitional Commercial District is located primarily between 6th and 7th Avenue east of the traditional Central Business District. There is a commercial focus to the District with an intent to encourage pedestrian oriented development yet provide for limited off-street parking. Buildings are encouraged to be close to the front and side property lines with parking being directed to the center of the lot. Development in the district is encouraged to provide frontage to 6th and 7th Avenues.



Key	
-----	PROPERTY LINE
- - - -	SETBACK LINE
■	BUILDING ENVELOPE

Building Placement

Setback (Distance from Property Line)

Front	5' min.; 15' max.	A
Side Street, Corner Lot	5' min.; 15' max.	B
Side	0'	C
Rear		
Adjacent to Residence	15'	D
Adjacent to any other use	10'	D

Building Form

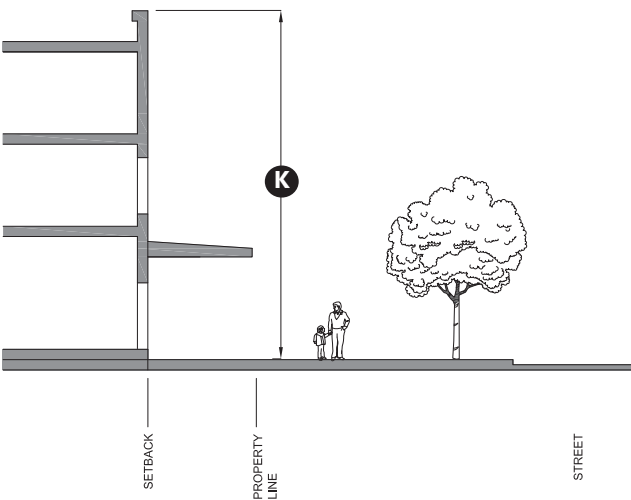
Primary Street - Bldg. Frontage on Setback	60%	E
Lot Width	300' max.	G
Lot Depth	N/A	H
Lot Coverage	70% max.	

Accessory Buildings

Front Setback	20' plus front yard
Side Setback	5' min. (or existing building setback)
Rear Setback	5' min.; 10' to an alley

Notes

N/A



Height

Building Maximum	4 stories	K
Accessory Building Max.	1 story	

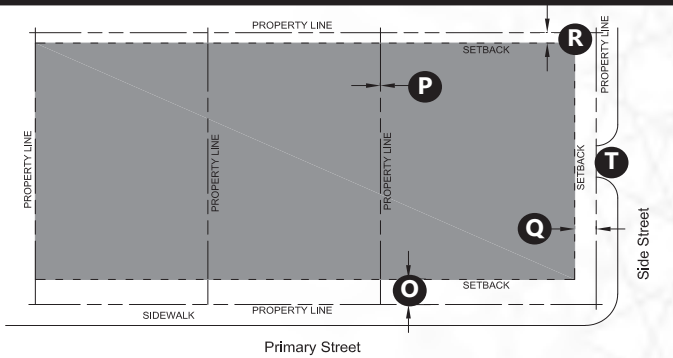
Notes

All floors must have a primary ground-floor entrance that faces the primary or side street.

Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.

Mansard roof forms are not allowed.

Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.



Key	
-----	PROPERTY LINE
■	PARKING AREA

Parking

Location (Distance from Property Line)

Front Setback	10'	O
Side Setback	0'	P
Side Street Setback	10'	Q
Rear Setback	5'	R
Adjacent Residential	15' w/buffer	R

Required Parking Spaces*

Multi-Unit, Upper Level Dwellings	1.5 space/unit
Commercial/Retail/Service	3.5 space/1,000 GSF
Medical Office	1.0 space/150 GSF
Lodging	1.0 space/room
Bicycle Parking	1.0 space/10 vehicle space

* Reference Section 176.29-2 Parking and Loading for uses not specified; Parking lot screening per Section 176.29-2.H.(7); For Mixed Use developments, parking requirements can be reduced by the shared parking factor.

Notes

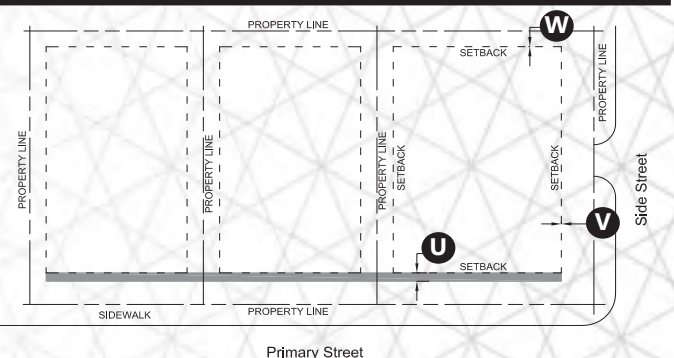
On corner lots, primary parking drive shall not be located on primary street. T

Parking not allowed between primary building and curb.

Connectivity is encouraged between adjacent lots.

Parking only allowed to the back or side of building or in parking garage which is to be located to the back or side of building.

Landscape buffer is required between UTC-2 and adjacent residential.



Key	
-----	PROPERTY LINE
- - - -	SETBACK LINE
■	ENCROACHMENT AREA

Setback Encroachments

Location

Front	8' max.	U
Side Street	0' max.	V
Rear	0' max	W

Notes

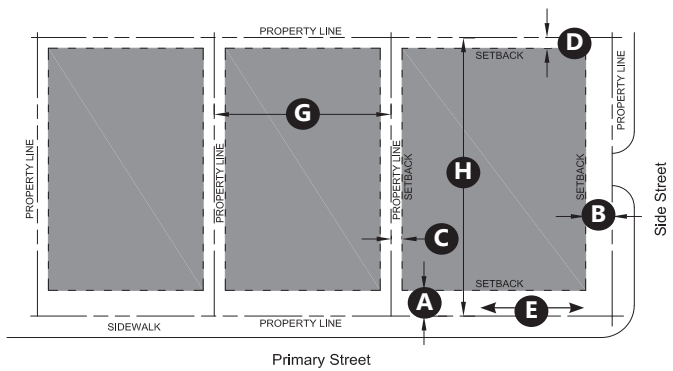
Porches, commercial storefronts, balconies, bay windows, stoops and awnings, may encroach into the setback, as shown in the shaded areas.

Frontage Types

Forecourts		
	Depth	20' min., not to exceed width
	Width	20' min., 50% of lot width max.
<hr/>		
Stoops		
	Depth	4' min., 6' max.
<hr/>		
Storefront		
	Depth	0' min., 6' max.

URBAN COMMERCIAL 1 (UC-1) DISTRICT STANDARDS

The UC-1 Urban Commercial District is similar to Marion’s current C-3 General Commercial zoning district. Located on the east end of Marion’s Central Corridor the District serves as a transition district to the standard zoning districts east of the Central Corridor.



Key

PROPERTY LINE

SETBACK LINE

BUILDING ENVELOPE

Building Placement

Setback (Distance from Property Line)		
Front	10' min.	A
Side Street, Corner Lot	10' min.	B
Side	10' min.	C
Rear		
Adjacent to Residence	25'	D
Adjacent to any other use	5'	D

Building Form

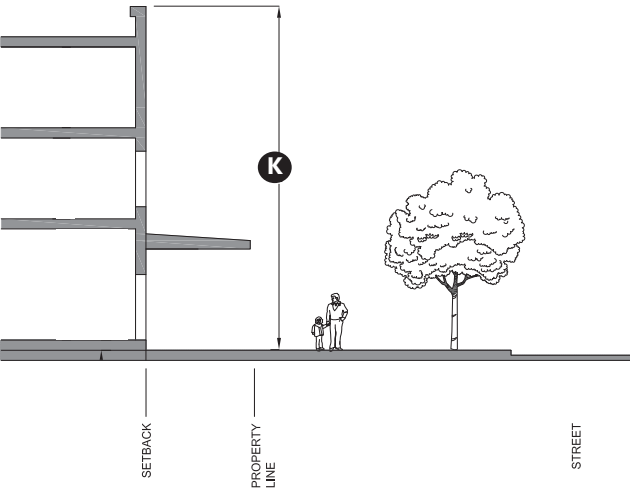
Primary Street - Bldg. Frontage on Setback	60%	G
Lot Width	150' min.	H
Lot Depth	N/A	
Lot Coverage	70%	

Accessory Buildings

Front Setback	20' plus front yard
Side Setback	5' min. or existing building setback
Rear Setback	5' min.; 10' to an alley

Notes

N/A



Height

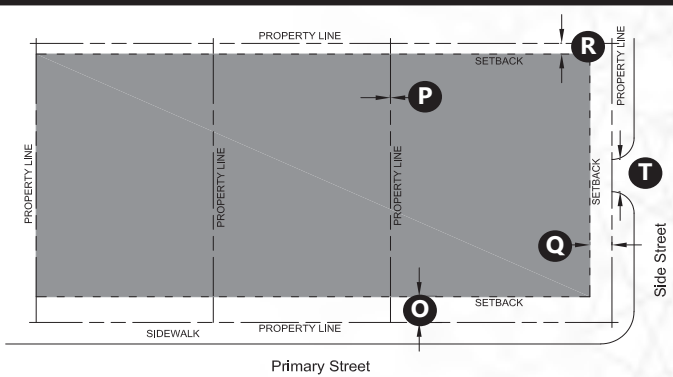
Building Maximum	4 stories	K
Accessory Building Max.	1 story	

Notes

Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.

Mansard roof forms are not allowed.

Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.



Key

PROPERTY LINE

PARKING AREA

Parking

Location (Distance from Property Line)		
Front Setback	10'	O
Side Setback	0'	P
Side Street Setback	10'	Q
Rear Setback	5'	R
Adjacent Residential	15' w/buffer	R

Required Parking Spaces*

Upper Level Dwellings	1.5 space/unit
Commercial/Retail/Service	3.5 space/1,000 GSF
Medical Office	1.0 space/150 GSF
Lodging	1.0 space/room
Bicycle Parking	1.0 space/10 vehicle space

* Reference Section 176.29-2 Parking and Loading for uses not specified; Parking lot screening per Section 176.29-2.H.(7); For Mixed Use developments, parking requirements can be reduced by the shared parking factor.

Notes

On corner lots, primary parking drive shall not be located on primary street.

Connectivity is encouraged between adjacent lots.

Landscape buffer is required between UC-1 and adjacent residential.

CENTRAL CORRIDOR DESIGN STANDARDS

Private Frontage Types

Common Yard: A planted frontage wherein the building is setback substantially from the property line/right-of-way line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The deep setback provides a buffer from the higher speed thoroughfares.

Porch: A planted frontage where the building is setback from the property lin/right-of-way line with an attached porch permitted to encroach.

Terrace: A frontage wherein the building is setback from the property line/right-of-way line by an elevated terrace. This type buffers residential uses from urban sidewalks and removes the private yard from public encroachment. Terraces are suitable for conversion to outdoor cafes.

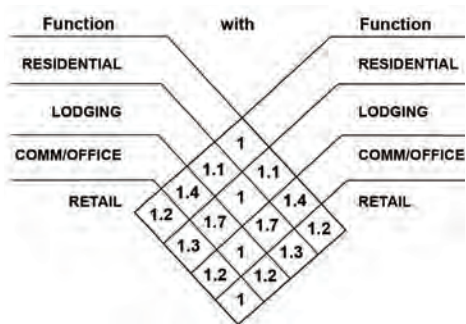
Forecourt: A frontage wherein the building is close to the property line/right-of-way line and the central portion is set back. The forecourt created is suitable for vehicular drop-offs. This type should be located in conjunction with other Frontage types. Large trees within the forecourts may overhang the sidewalk.

Stoop: A frontage wherein the building is aligned close to the property line/right-of-way line with the first story elevated from the sidewalk sufficiently to ensure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential uses.

Storefront: A frontage wherein the building is aligned close to the property line/right-of-way line with the building entrance at sidewalk grade. This type is conventional for retail uses. It has substantial glazing on the sidewalk level and an awning that should overlap the sidewalk.

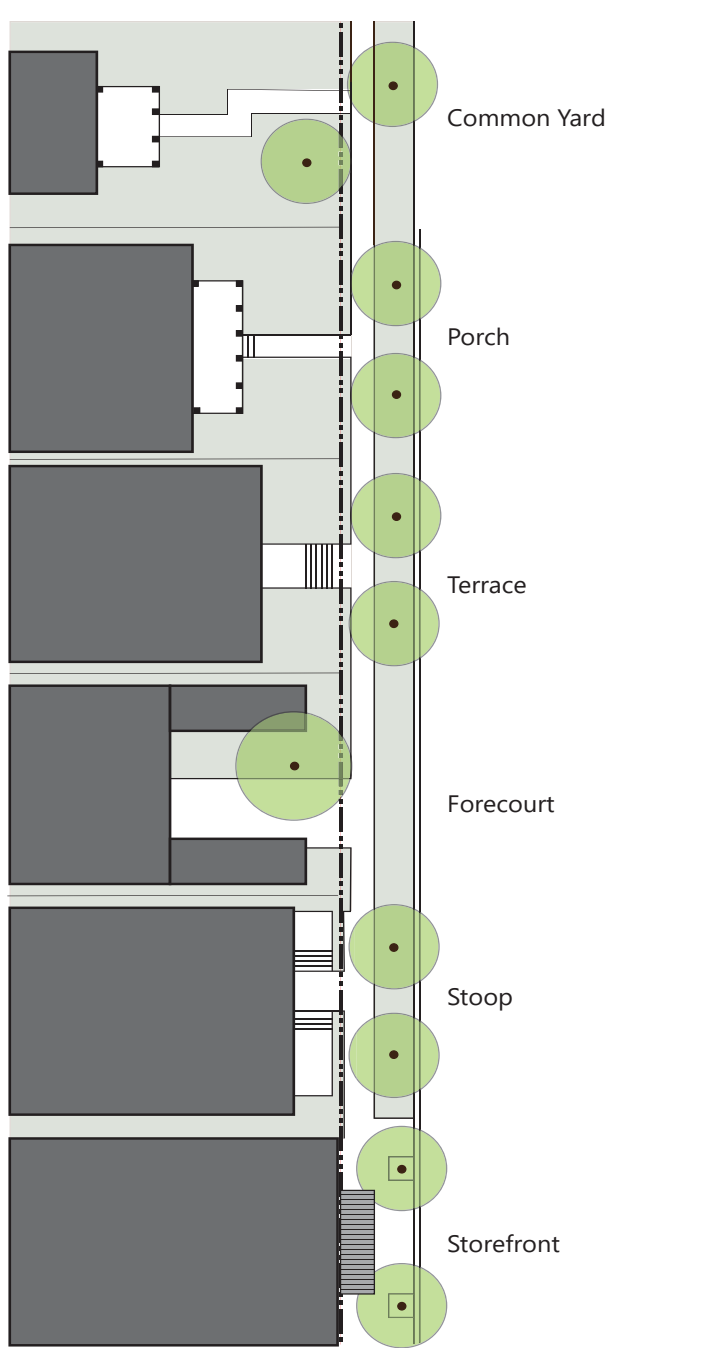
Shared Parking Factor

For Mixed Use developments, parking requirements can be reduced by the shared parking factor.



The Shared Parking Factor reduces the required number of parking spaces required when calculated each separate use, i.e. residential and comm/office, and dividing it by the Shared Parking Factor such as 1.4 in the case of a residential and comm/office mixed use building. The uses must be in the same building.

Private Frontage Types



Other Provisions

Landscape Regulations	Section 176.43
Temporary Uses Regulations	Section 176.35

Architecture

In the interest of promoting the general welfare of the community and to protect the value of buildings and property, the image and character of a community is considered important. It is recognized that the community should be visually attractive, as well as financially prosperous and the manner in which a use is accomplished is as important as the use. The quality of architecture and building construction is important to the preservation and enhancement of building and property values, prevention of the physical deterioration of buildings and the promotion of the image of the community and the general welfare of its citizens. Architectural design and use of materials for the construction of any building shall be subject to the approval of the City Council.

Consideration of exterior building materials on all sides, coloration of materials, building height, roof line, size and location of windows and doors, roof mounted appurtenances, and facades.

Design Guidelines

Proportion
The relationship of width and height of the front elevations of adjacent buildings shall be considered in the construction or alteration of a building. The relationship of width and height of windows and doors of adjacent buildings shall be considered in the construction or alteration of a building. Particular attention must be given to the scale of street level doors, walls and windows. Large expanses of blank wall spaces at street level are to be discouraged.

Coloration
Building colors should emphasize light and muted colors, with light earth tones dominant. Saturated hues and bright colors except for use in small areas is not encouraged.

Architectural Features
These features include, but are not limited to cornices, entablatures, doors, windows, shutters, fanlights, and other elements prevailing in the area shall be considered in the construction or alteration of a building. It is not intended that the details of existing buildings be duplicated precisely, but those features should be suggestive of the extent, nature, and scale of details that would be appropriate on new buildings or associated with building alterations.

Exterior Facade Materials

Exterior Facade Materials	
Primary Material	75% of the wall area (excluding glass).
Primary Exterior Material	Fired Clay brick, natural stone, glass, architectural concrete panels, textured concrete block, or architectural steel, or stone panels.
Trim	Not to exceed 25% (excluding glass)
Trim Includes	Moldings, cornices, parapet, frieze, sills, lintels, string-course, quoining, and ledge ment.

Proposed new buildings, additions, and renovations of buildings are subject to the Central Corridor District Standards.

No wood, Masonite, asphaltic exterior wall or roof material, aluminum or steel siding, non-architectural sheet metal, on-textured concrete block, stucco, vinyl, E.I.F.S. (Exterior Insulation and Finish Systems) or other similar materials shall constitute a portion of any building, except as trim.

In order to achieve continuity along this corridor, buildings abutting or within 300 feet of 7th Avenue/Business 151 or 6th Avenue, shall be designed in conformance with the guidelines herein.

Unsightly/utilitarian aspects of buildings (loading docks, loading areas, outside storage areas, garbage dumpsters and HVAC mechanical units) shall be screened from any public street and adjoining properties. Preference is given to siting loading docks or loading areas out of view from public streets.

Sign Regulations

All signage shall conform to requirements of Chapter 176.31 Signs, except as provided for below:

Electric Message Center Signs shall not be permitted within the Central Corridor.

PERMITTED USES

Zoning Districts -- Permitted and Special Uses								
P = Permitted Use C = Conditional Use A = Permitted as Accessory Use	U-1	U-2	UTR-1	UTR-2	UTC-1	UTC-2	UC-1	Specific Use Standards
RESIDENTIAL USES								
Artist Live/Work Space	P	P						Y
Assisted Living Facility				P				
Dwelling Unit, Auxiliary (attached)			P	P				Y
Dwelling Unit, Auxiliary (detached)			P	P				Y
Dwelling, Efficiency/Micro	P	P						
Dwelling, One Unit			P	P				
Dwelling, Townhouse			P	P				
Dwelling, Two-unit (duplex)			P	P				
Dwelling, Three-unit (triplex)			P	P				
Dwelling, Four-unit			P	P				
Dwelling, Multi-Unit				P	P	P		
Dwelling, Upper level	P	P		P	P	P	P	
Family Home		P	P					
Group Home, Large			P					Y
Group Home, Small			P					Y
Independent Living Facility				P				
RETAIL AND SERVICE USES								
Bank	P	P			P	P	P	
Bed and Breakfast			C					Y
Car Wash							P	Y
Coffee or Tea Room			C					
Day Care Center		C			C	C		
Delayed Deposit						C	C	Y
Drive-Thru Facility						C	P	Y
Financial Institution	P	P			P	P	P	
Firework Sales								
Funeral Home / Mortuary								
Gas Station						C	C	Y
Grocery						P	P	
Grocery - Neighborhood	P	P			P	P	P	
Heavy Retail and Service								
Home Improvement Center								
Hotel/Motel	P	P			P	P	P	
Kennel								Y
Medical Cannabidiol Dispensaries (Sales)								Y
Medical/Dental Clinic		P			P	P	P	
Medical/Dental Clinic, Not exceeding 2500 gross floor area.	P	P			P	P	P	
Microbrewery/Micro-Distillery/Micro-Winery	P	P				P	P	

Zoning Districts -- Permitted and Special Uses								
P = Permitted Use C = Conditional Use A = Permitted as Accessory Use	U-1	U-2	UTR-1	UTR-2	UTC-1	UTC-2	UC-1	Specific Use Standards
Motor Vehicle Rental								
Motor Vehicle Sales and Leasing								Y
Motor Vehicle Service and Repair, Major								Y
Motor Vehicle Service and Repair, Minor								Y
Office, Business and Professional	P	P			P	P	P	
Outdoor Sales, Permanent								Y
Outdoor Sales, Temporary	A	A			A	A	A	Y
Pawn Shop					P	P	P	
Personal Services	P	P			P	P	P	
Personal Services, Limited	P	P			P	P	P	
Professional Training Center	P	P			P	P	P	
Restaurant	P	P			P	P	P	
Retail Sales Not exceeding 2,500 gross floor area	A	A		C	A	A	A	
Retail Sales Large								
Retail Sales, Medium						P	P	
Retail Sales, Small	P	P			P	P	P	
Retail Sales, Specialty	P	P			P	P	P	
Sexually Oriented Business								Chapter 127, Code of Ord.
Tattoo Parlor / Body Piercing Studio	P	P			P	P	P	
Tavern/Bar	P	P			P	P	P	
Veterinary Office/Animal Hospital						P	P	Y
CULTURAL, RELIGIOUS, RECREATIONAL AND ENTERTAINMENT USES								
Art Gallery/Studio	P	P			P	P	P	
Carnival (as temporary use)								Y
Convent/Monastery			C					
Cultural Facility	P	P			P	P	P	
Golf Course								
Indoor Recreation and Amusement	P	A			P	P	P	
Live Entertainment	A	A			A	A	A	
Lodge or Private Club	P	P			P	P	P	
Outdoor Dining	P	P			P	P	P	
Outdoor Amusement								
Outdoor Recreation								
Outdoor Entertainment, Temporary	P	P			P	P	P	
Park, Neighborhood								
Place of Worship	C	C	C	C	C	C	C	
Public Plaza	P	P			P	P	P	
Theater (Small/Large)	P	P			P	P	P	

PERMITTED USES CONTINUED

Zoning Districts -- Permitted and Special Uses								
P = Permitted Use C = Conditional Use	U-1	U-2	UTR-1	UTR-2	UTC-1	UTC-2	UC-1	Specific Use Standards
A = Permitted as Accessory Use								
GOVERNMENTAL AND INSTITUTIONAL USES								
Cemetery								
College/University		P				P	P	
Correctional Facility								
Emergency Medical Center							P	
Fairground								
Homeless Shelter			C					
Hospice								
Hospital							P	
Library	P	P			P	P	P	
Nursing Home								
Office, Government	P	P			P	P	P	
Post Office							P	
Police Firearms Training Range								
Public Service Facility	C	C			C	C	C	
School, Specialized Instructional	P	P			P	P		
School, Primary or Secondary			C	C				
OTHER								
Horse Stables, Private								
Parking Garage/Structure	P	P			C	C	C	
Parking Lot, Private	C	C			A	A	A	
Parking Lot, Public	P	P			P	P	P	
Transportation Operations Facility						C	C	
Communication Tower								Chapter 176.40, Code of Ord.
Communication Antenna	A	A			A	A	A	Chapter 176.40, Code of Ord.
Urban Agricultural	A	A	A	A	A	A	A	
Utility, Community/Regional	C	C			C	C	C	
Utility, Local	C	C	C	C	C	C	C	
Wind Turbine, Structure Mounted								Chapter 176.46, Code of Ord.
Wind Turbine, Tower Mounted	C	C			C	C	C	Chapter 176.46, Code of Ord.

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